

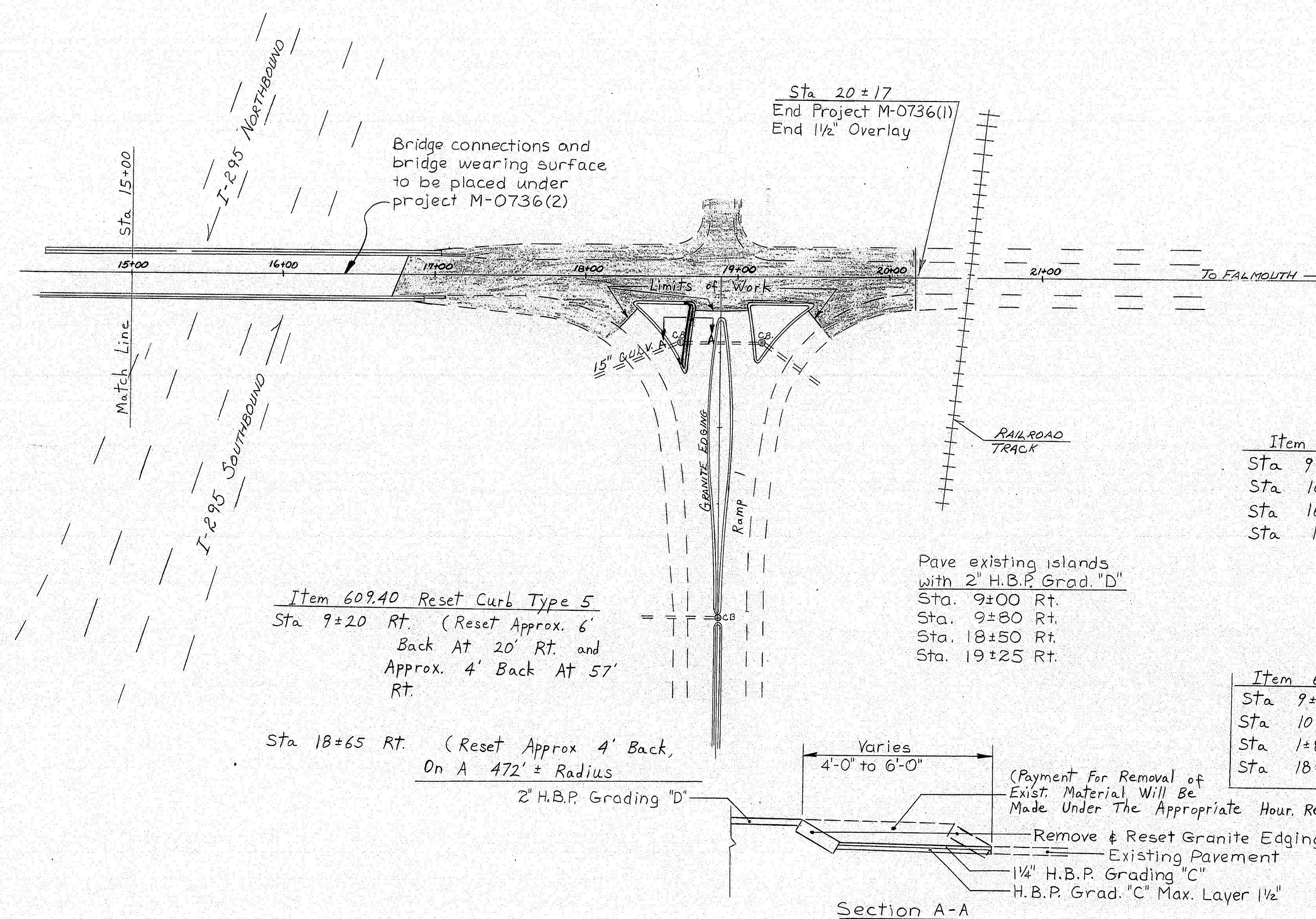
BUCKNAM ROAD INDEX OF SHEETS

BUCKNAM ROAD PLAN	1
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ESTIMATED QUANTITIES	3
BRIDGE GENERAL PLAN	4
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F.R.W.A. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	M-0736(1)	1	23
		M-0736(2)		

INDEX OF SHEETS

BUCKNAM ROAD PLANS	1-7
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MAINTENANCE OF TRAFFIC	13-15
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Item 606.364 Guard Rail, Remove, Modify And Reset
Sta 9+62 To 14+37 Lt
Sta 10+25 To 14+25 Rt
Sta 16+91 Rt To 1+86, Rt Ramp 1
Sta 17+05 To 18+30 Lt

Item 606.77 Breakaway Cable Terminal
Sta 9+57 To 9+62 Lt
Sta 10+0 To 10+25 Rt
Sta 1+86 To 2+11, Rt Ramp 1
Sta 18+30 To 18+55 Lt

Item 609.40 Reset Curb Type 5
Sta 9+20 Rt. (Reset Approx. 6'
Back At 20' Rt. and
Approx. 4' Back At 57'
Rt.

Sta 18+65 Rt. (Reset Approx 4' Back,
On A 472' ± Radius
2" H.B.P. Grading "D"

Pave existing islands
with 2" H.B.P. Grad. "D"
Sta. 9+00 Rt.
Sta. 9+80 Rt.
Sta. 18+50 Rt.
Sta. 19+25 Rt.

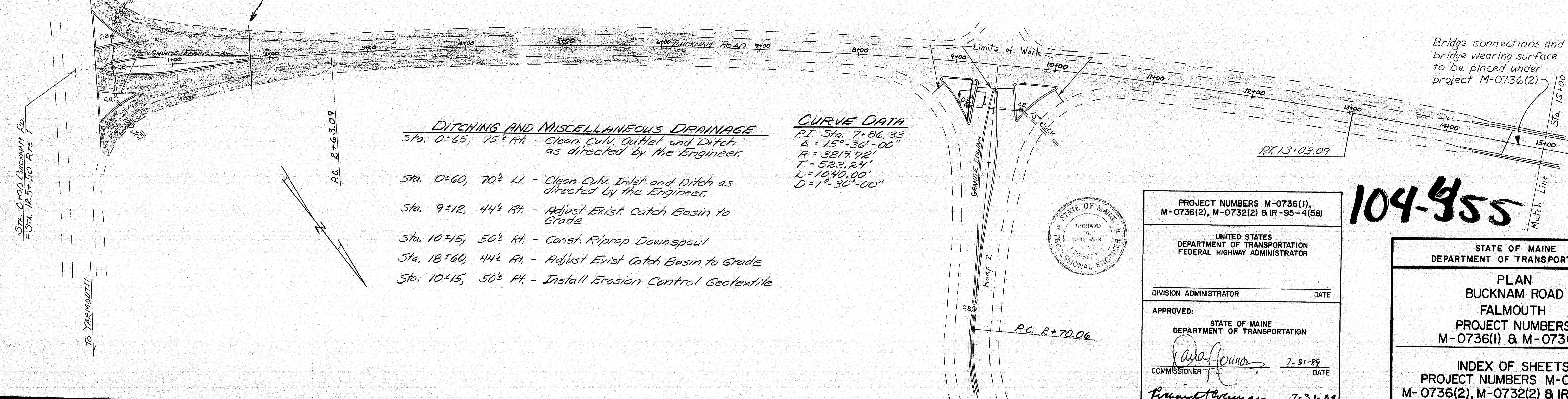
(Payment For Removal of
Exist. Material Will Be
Made Under The Appropriate Hour. Rental Items.)

Remove & Reset Granite Edging
Existing Pavement
1 1/4" H.B.P. Grading "C"
H.B.P. Grad. "C" Max. Layer 1 1/2"

Section A-A

G19

Sta 1+80 Lt and Rt
Begin Project M-0736(1)
Begin 1 1/2" Overlay



DITCHING AND MISCELLANEOUS DRAINAGE
Sta. 0+65, 75' Rt. - Clean Cully Outlet and Ditch
as directed by the Engineer.

Sta. 0+60, 70' Lt. - Clean Cully Inlet and Ditch as
directed by the Engineer.

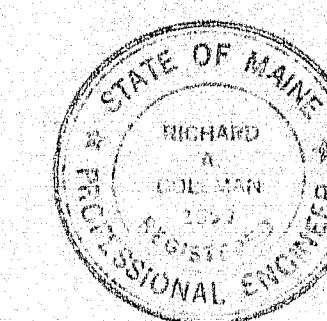
Sta. 9+12, 44 1/2' Rt. - Adjust Exist. Catch Basin to
Grade

Sta. 10+15, 50 1/2' Rt. - Const. Riprap Downspout

Sta. 18+60, 44 1/2' Rt. - Adjust Exist. Catch Basin to Grade

Sta. 10+15, 50 1/2' Rt. - Install Erosion Control Geotextile

CURVE DATA
P.I. Sta. 7+86.33
L = 15' 36' 00"
R = 3819.92'
T = 523.24'
L = 1040.00'
D = 1° 30' 00"

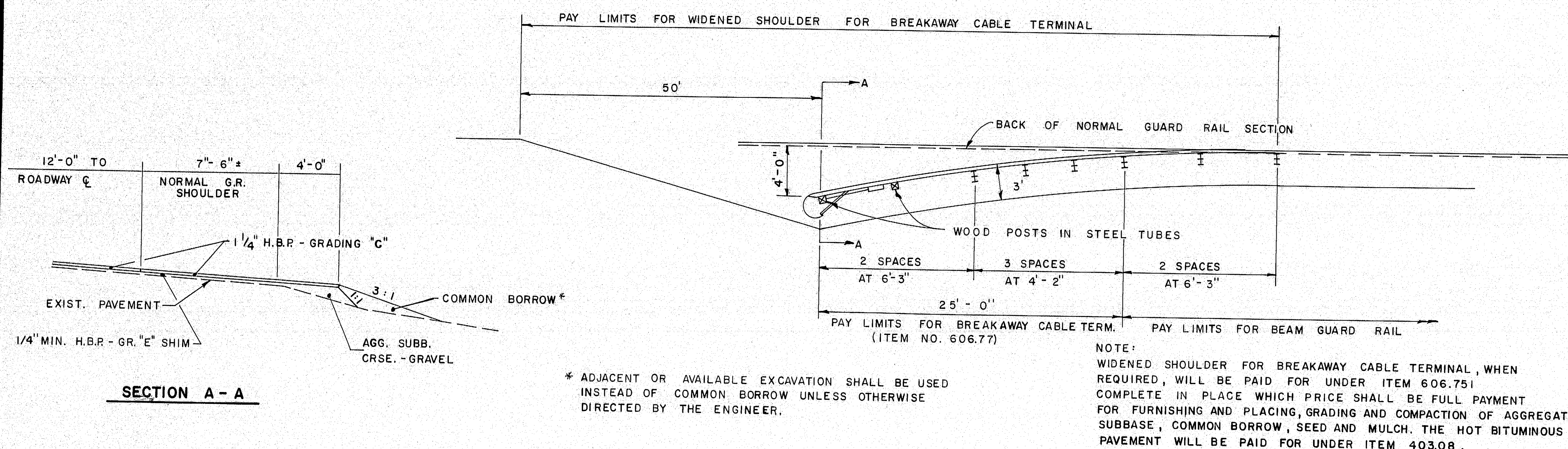
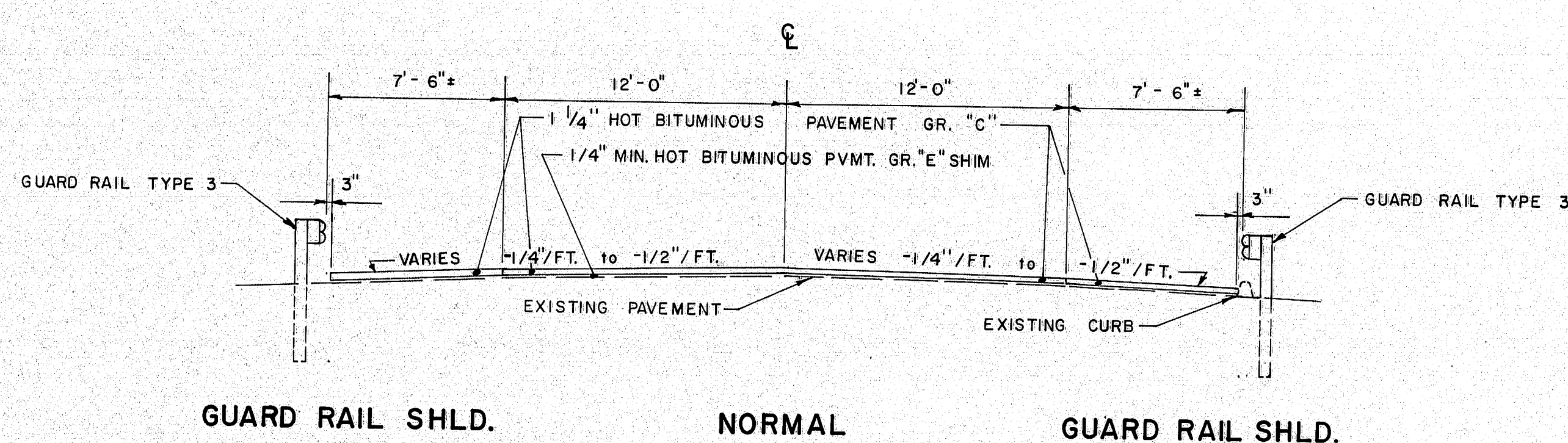
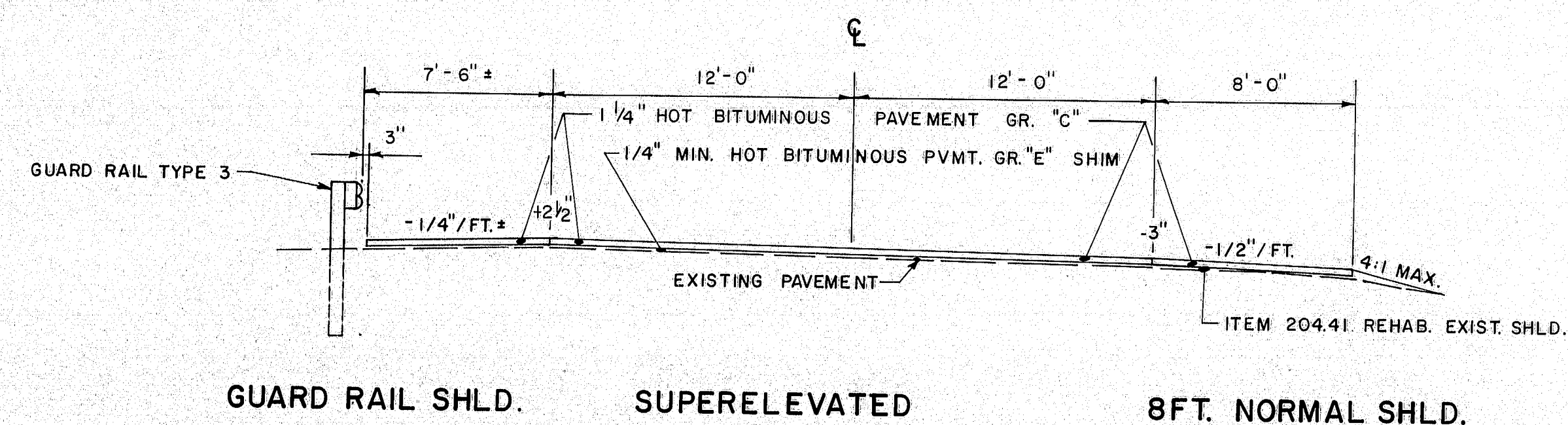


PROJECT NUMBERS M-0736(1), M-0736(2), M-0732(2) & IR-95-4(58)	
UNITED STATES DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATOR	
DIVISION ADMINISTRATOR	DATE
APPROVED: STATE OF MAINE DEPARTMENT OF TRANSPORTATION	
COMMISSIONER	7-31-89 DATE
CHIEF ENGINEER	7-31-89 DATE

104-955

STATE OF MAINE DEPARTMENT OF TRANSPORTATION	
PLAN BUCKNAM ROAD FALMOUTH PROJECT NUMBERS M-0736(1) & M-0736(2)	
INDEX OF SHEETS PROJECT NUMBERS M-0736(1), M-0736(2), M-0732(2) & IR-95-4(58)	
SHEET 1 OF 7 AUGUSTA, MAINE August 1989	

F.H.W.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	M-0786(1)	2	23



General Notes

- THE UTILITIES INVOLVED IN THIS CONTRACT ARE:
NORTHERN UTILITIES(GAS)
TOWN OF FALMOUTH(SEWER)
CENTRAL MAINE POWER COMPANY
NEW ENGLAND TELEPHONE COMPANY
PUBLIC CABLE
St. Lawrence & Atlantic Railroad Company
PORTLAND WATER DISTRICT
ALL UTILITY FACILITIES SHALL BE ADJUSTED BY THE RESPECTIVE UTILITIES UNLESS NOTED.
- THE ENGINEER WILL DESIGNATE UNSAFE RECOVERY AREAS AT THE TOES OF NON-GUARDRAIL FILL SLOPES TO BE GRADED BY BULLDOZER AND/OR OTHER HOURLY RENTAL ITEMS. BOULDERS, LARGE STUMPS AND OTHER OBJECTS SHALL BE BURIED OR REMOVED. THE USE OF BORROW OR WASTE MATERIAL MAY BE AUTHORIZED FOR SOME AREAS. UPON COMPLETION OF THE GRADING, THE AREAS SHALL BE SEEDDED WITH METHOD NUMBER 2 AND MULCHED.
- ONE GUARDRAIL DELINEATOR POST SHALL BE INSTALLED AT EACH GUARD RAIL END AND UNDERDRAIN OUTLET.
- CURB TYPE 3 TO BE INSTALLED WITH MOLD 2 AND SEALED WITH BITUMINOUS SEALING BLACK, WHEN DIRECTED.
- ACRYLIC LATEX COLOR FINISH GREEN SHALL BE PLACED ON ALL ISLANDS NOTED TO BE PAVED.
- BREAKAWAY CABLE TERMINALS SHALL BE INSTALLED CONCURRENTLY WITH THE PLACEMENT OF EACH SECTION OF BEAM GUARDRAIL.
- ALL JOINTS BETWEEN EXISTING AND PROPOSED HOT BITUMINOUS PAVEMENT SHALL BE BUTTED, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. PAYMENT TO BE INCIDENTAL TO ITEM 403.
- A 3' PAVED LIP SHALL BE PLACED AT ALL GRAVEL ENTRANCES EXCEPT WOODS AND FIELD ENTRANCES UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- EXISTING CULVERTS TO REMAIN SHALL BE CLEANED AS DIRECTED BY THE ENGINEER. PAYMENT WILL BE MADE UNDER ITEM 631.32 CULVERT CLEANER (INCLUDING OPERATOR).
- ANY NECESSARY CUTTING OF CURB TO BE REMOVED AND RESET WILL NOT BE PAID FOR SEPARATELY AND WILL BE CONSIDERED INCIDENTAL TO ITEM 609.
- DITCHING, AS DIRECTED BY THE ENGINEER, SHALL BE PAID FOR UNDER HOURLY RENTAL ITEMS.
- ANY EXISTING PAVED SIDE ROADS OR SHOULDERS SHALL BE RESURFACED AS DETERMINED IN THE FIELD BY THE ENGINEER.
- EXCESS TERMINAL ENDS AND GUARDRAIL SHALL BE REMOVED AND STOCKPILED FOR PICKUP BY MAINTENANCE PERSONNEL. NO SEPARATE PAYMENT WILL BE MADE FOR REMOVAL AND STOCKPIILING.
- LOAM, SOD, TEMPORARY EROSION CONTROL BLANKET, SEEDING AND MULCH HAVE BEEN ESTIMATED FOR UNDETERMINED LOCATIONS AND SHALL BE PLACED WHEN DIRECTED BY THE ENGINEER.
- LOAM DEPTHS ARE 2" AND ARE CONSIDERED NOMINAL.
- PORTIONS OF THE EXISTING BITUMINOUS CURB ARE MISSING AND SHALL BE REPLACED WITH CURB TYPE 3 AS DIRECTED BY THE ENGINEER.
- WIDEN SHOULDER FOR BREAKAWAY CABLE TERMINAL HAVE BEEN ESTIMATED FOR EACH BCT LOCATION. HOWEVER, IF THE PRESENT SHOULDER IS WIDE ENOUGH TO ACCOMMODATE A BCT, THIS ITEM WILL BE ELIMINATED AS DIRECTED BY THE ENGINEER.

104-856

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

GENERAL NOTES

MISCELLANEOUS DETAILS

SHEET 2 OF 7 AUGUSTA, MAINE August 1989

Falmouth Bucknam Rd. Revised As-Built Jeffrey Peterson 3/91

ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	QUANTITY	UNIT
202.127	REMOVAL OF EXISTING BITUMINOUS PAVEMENT	0.24	L.S.
204.41	REHAB. EXISTING SHOULDER, PLAN QUANTITY.	1100	S.Y.
304.10	AGGREGATE SUBBASE COURSE - GRAVEL	200	C.Y.
403.08	HOT BITUMINOUS PAVEMENT, GRADING C.	650	T.
403.10	HOT BITUMINOUS PAVEMENT, GRADING D.	107	T.
403.101	HOT BIT PAVE, GRADD(SDWLKS, SHMS, DRIVES, INCD).	70	T.
403.121	HOT BITUMINOUS PAVEMENT, GRADING E (SHIMMING).	350	T.
409.15	BITUMINOUS TACK COAT, APPLIED.	150	G.
503.12	REINFORCING STEEL FAB & DELIVERED.	220	L.B.
503.13	REINFORCING STEEL PLACING.	220	L.B.
504.700	STRUCTURAL STEEL FAB. & DELIVERED.	1	L.S.
504.710	STRUCTURAL STEEL ERECTION	1	L.S.
504.801	TEMPORARY SUPPORT SYSTEM - BUCKNAM ROAD	1	A.S.
506.142	FIELD PAINTING EXIST. STRUT STEEL	0.24	L.S.
508.13	MEMBRANE WATERPROOFING.	0.24	L.S.
515.21	PROTECTIVE COATING FOR CONCRETE SURFACES.	0.03	L.S.
518.21	REHAB. OF STRUCTURAL CONCRETE SUBSTRUCTURE.	155	S.F.
518.30	REHAB. OF STR. CONC. SLAB-TO REIN. STEEL.	294	S.F.
518.31	REHAB. OF STR CONC SLAB-TO BELOW REIN. STEEL.	118	S.F.
518.34	REHAB. OF CONC. CURBS & SIDEWALKS-INCLINED SURF.	168	S.F.
518.35	REHAB. OF CONC. CURBS & SIDEWALKS-HORIZ SURFACES	194	S.F.
520.2401	BRIDGE JOINT MODIFICATION - BUCKNAM ROAD	2	EA.
526.300	TEMPORARY CONCRETE BARRIER TYPE I	2200	L.F.
526.400	RESETTING TEMPORARY CONCRETE BARRIER TYPE I	600	L.F.
527.310	ENERGY APOSBLING SYSTEM (TEMP)	1	EA.
527.320	PORTABLE CRASH BARRELS	15	EA.
604.18	ADJUSTING MANHOLE OR CATCH BASIN TO GRADE	2	EA.
606.178	GUARD RAIL BEAM	100	L.F.
606.35	GUARD RAIL DELINEATOR POST	2	EA.
606.351	GUARD RAIL DELINEATOR POST REMOVE & RESET.	2	EA.
606.364	GUARD RAIL, REMOVE MODIFY AND RESET	1400	L.F.
606.367	REPLACE UNUSABLE EXISTING GUARD RAIL POST	10	EA.
606.751	WIDEN SHOULDER FOR BREAKWAY CABLE TERMINAL.	4	EA.

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STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

BUCKNAM ROAD
over
I-295

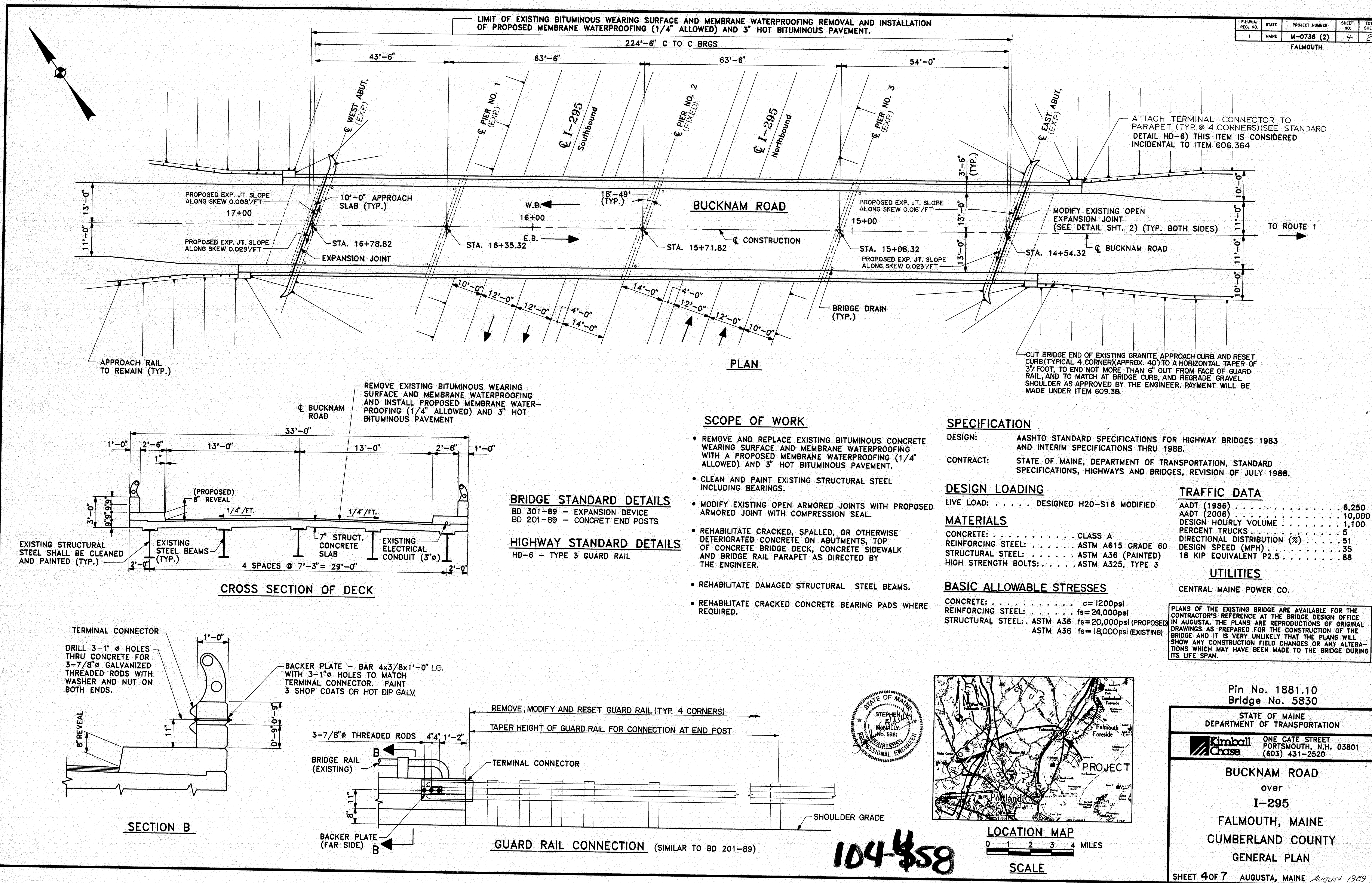
FALMOUTH, MAINE
CUMBERLAND COUNTY

ESTIMATED QUANTITIES

SHEET 3 OF 7 AUGUSTA, MAINE August 1989

REVISED As-BUILT Jeffrey Madson 3/91

89-1 2000

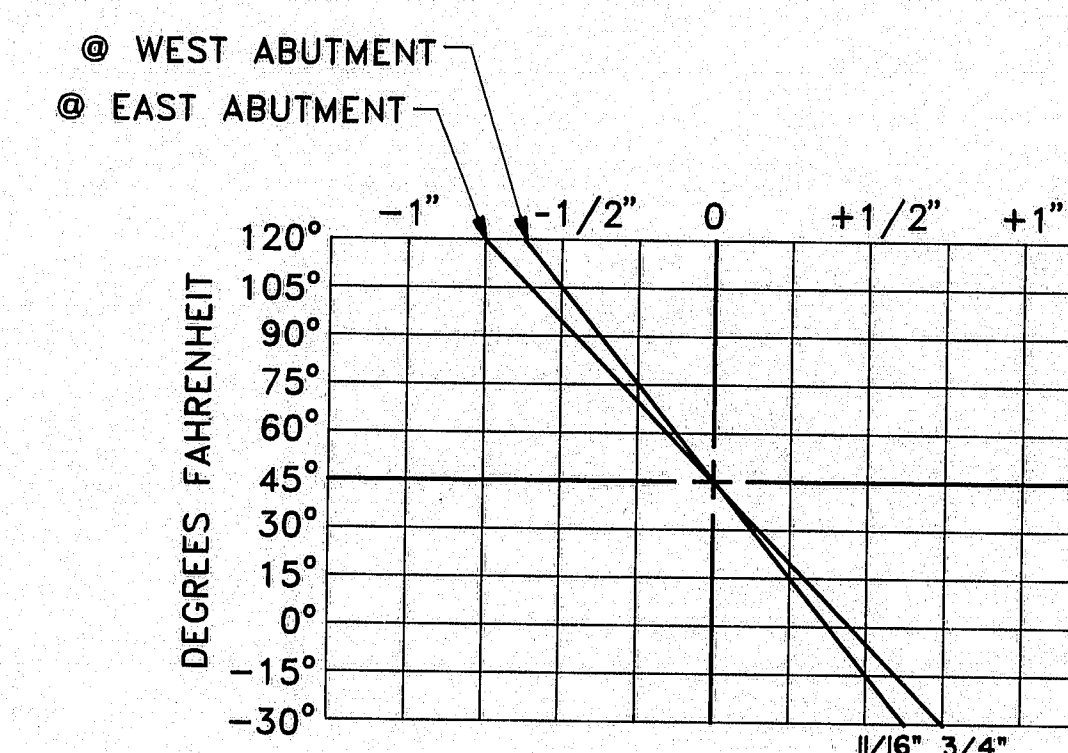
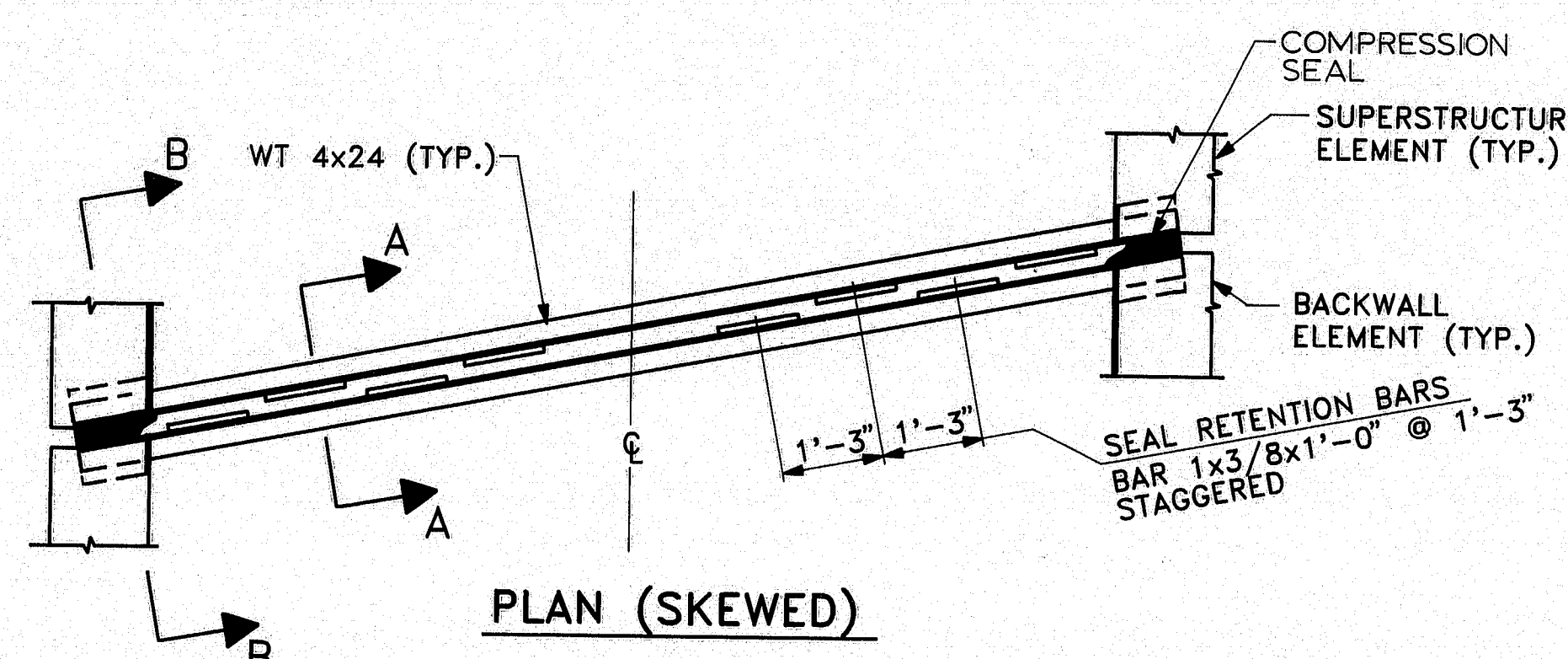
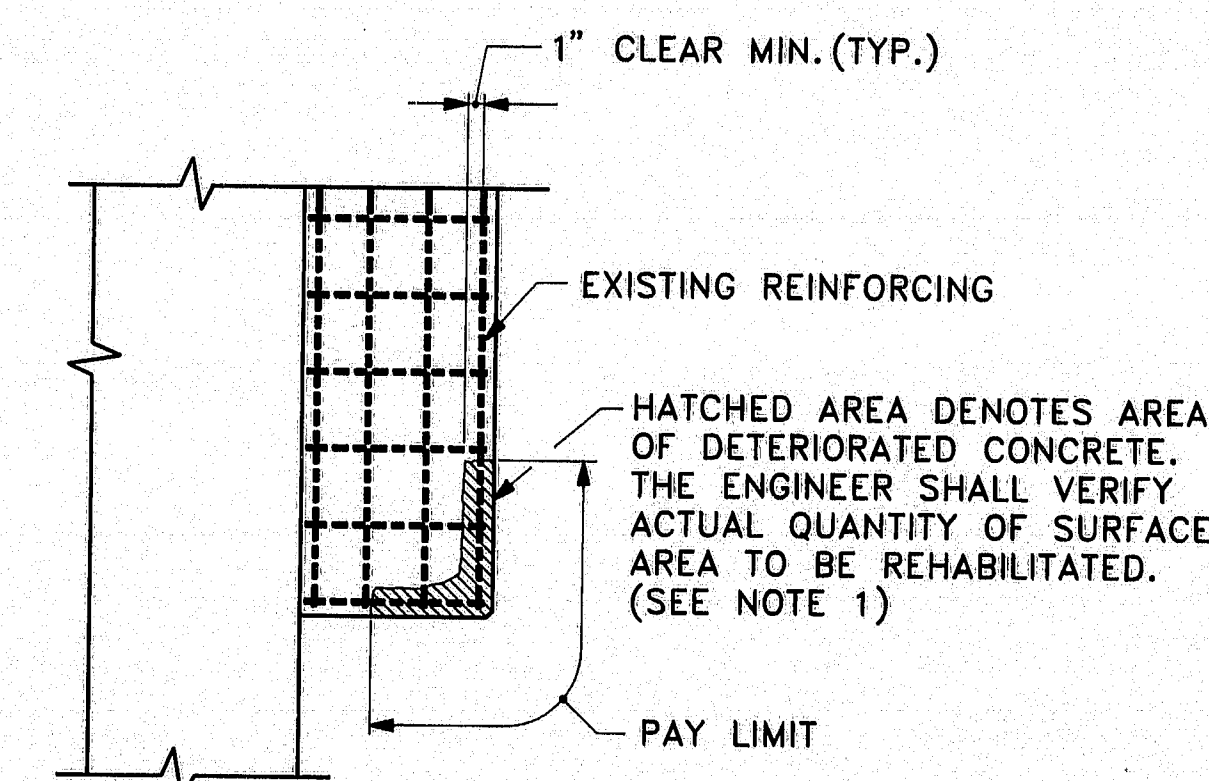
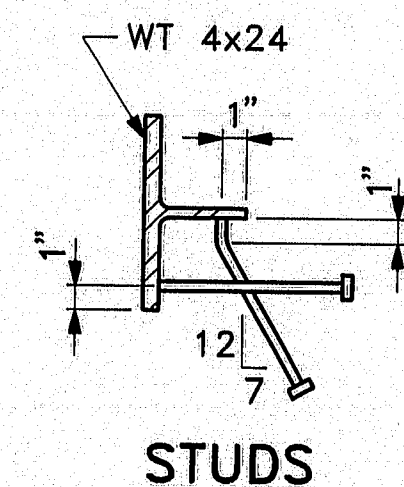
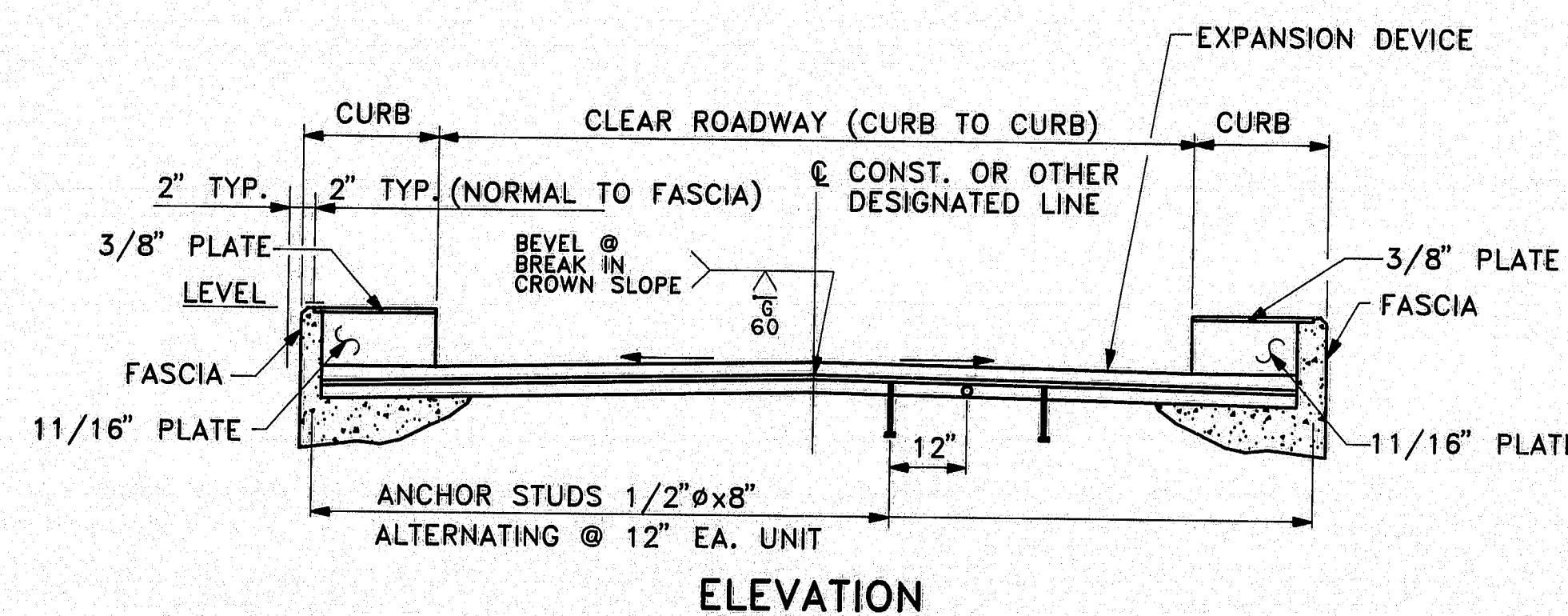


REVISED AS-BUILT Jeffrey Madson 3/9

F.A.M.A. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	M-0736 (2)	5	63
FALMOUTH				

THE TEMPORARY SIGNAL CONTROLLER SHALL BE A TWO-PHASE PRETIMED CONTROLLER. IT SHALL HAVE A FIRE PREEMPTION PHASE. WHEN ACTUATED IN THE FIRE STATION, THE CONTROLLER SHALL GO TO THE EASTBOUND PHASE AFTER NORMAL CLEARANCE INTERVALS. THE CONTRACTOR SHALL INSTALL A LINE TO THE FIRE STATION WITH A SWITCH TO ACTIVATE THE PREEMPTION PHASE.

SEQUENCE OF OPERATION							WHERE G=GREEN Y=YELLOW R=RED
INTERVAL	1	2	3	4	5	6	
HEADS							
EB	G	Y	R	R	R	R	G
WB	R	R	R	G	Y	R	Y
TIMING 80 SECOND DIAL	25	3	10	27	3	10	



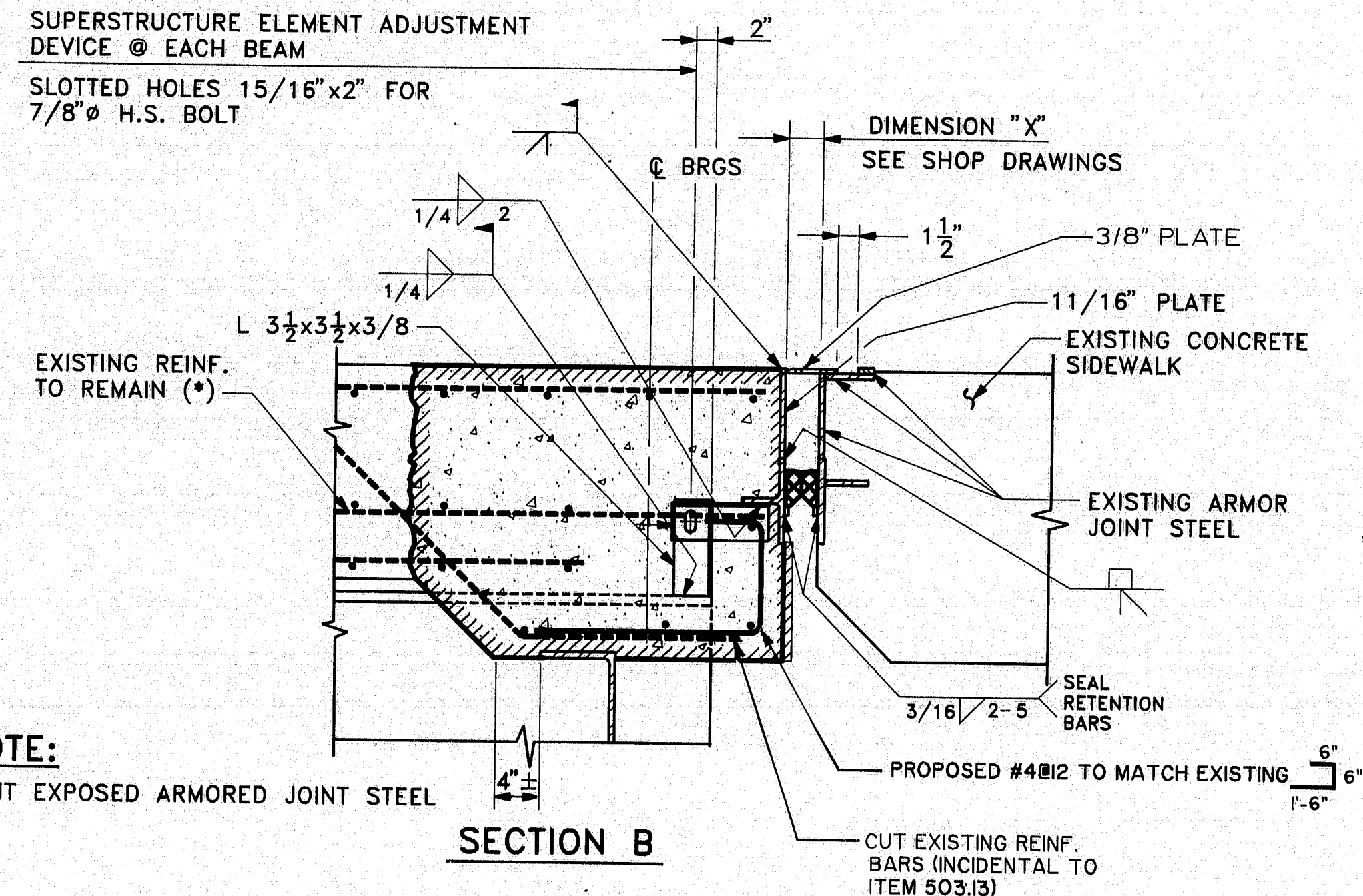
SUPERSTRUCTURE NOTES

- THE SEALS TO BE FURNISHED SHALL HAVE A MINIMUM MOVEMENT RATING OF:
WEST ABUTMENT: 1.375 INCHES
EAST ABUTMENT: 1.50 INCHES
- THE SEAL SHALL BE APPROVED BY THE ENGINEER PRIOR TO FABRICATION OF THE JOINT ARMOR.
- THE JOINT OPENING WILL VARY DEPENDING ON THE DIMENSIONS OF THE SEAL SELECTED BY THE CONTRACTOR. THE JOINT OPENING SHALL BE SET ACCORDING TO THE OPENING SHOWN ON THE APPROVED SHOP DETAIL DRAWINGS.
- THE COMPRESSION SEAL ADJUSTMENT CHART SHOWS THE ADJUSTMENT NECESSARY FOR THE JOINT OPENING SHOWN ON THE SHOP DETAIL DRAWINGS FOR TEMPERATURES OTHER THAN 45°F. ADJUSTMENT IS TO BE MEASURED PARALLEL TO THE CENTERLINE OF CONSTRUCTION.
- JOINT ARMOR SHALL BE FABRICATED AND DELIVERED TO THE SITE IN TWO (2) SECTIONS (SEE ELEVATION, THIS SHEET). INSTALLATION OF JOINT ARMOR SHALL BE PERFORMED TO ALLOW ONE LANE OF TRAFFIC OVER THE BRIDGE DURING CONSTRUCTION. THE TWO SECTIONS OF JOINT ARMOR ARE TO BE WELDED IN THE FIELD TO PRODUCE ONE CONTINUOUS SECTION. WELDS ARE TO BE GROUND SMOOTH AND PAINTED PRIOR TO COMPRESSION SEAL INSTALLATION.
- THE COMPRESSION SEALS SHALL BE INSTALLED IN ONE CONTINUOUS LENGTH EXTENDING THE FULL WIDTH OF THE BRIDGE, LESS ONE FOOT FROM THE OUTSIDE OF EACH FASCIA. SPLICING OF THE COMPRESSION SEAL IS NOT ALLOWED. A TEMPORARY INTERRUPTION OF TRAFFIC IS ANTICIPATED TO FACILITATE THE INSTALLATION.

NOTES

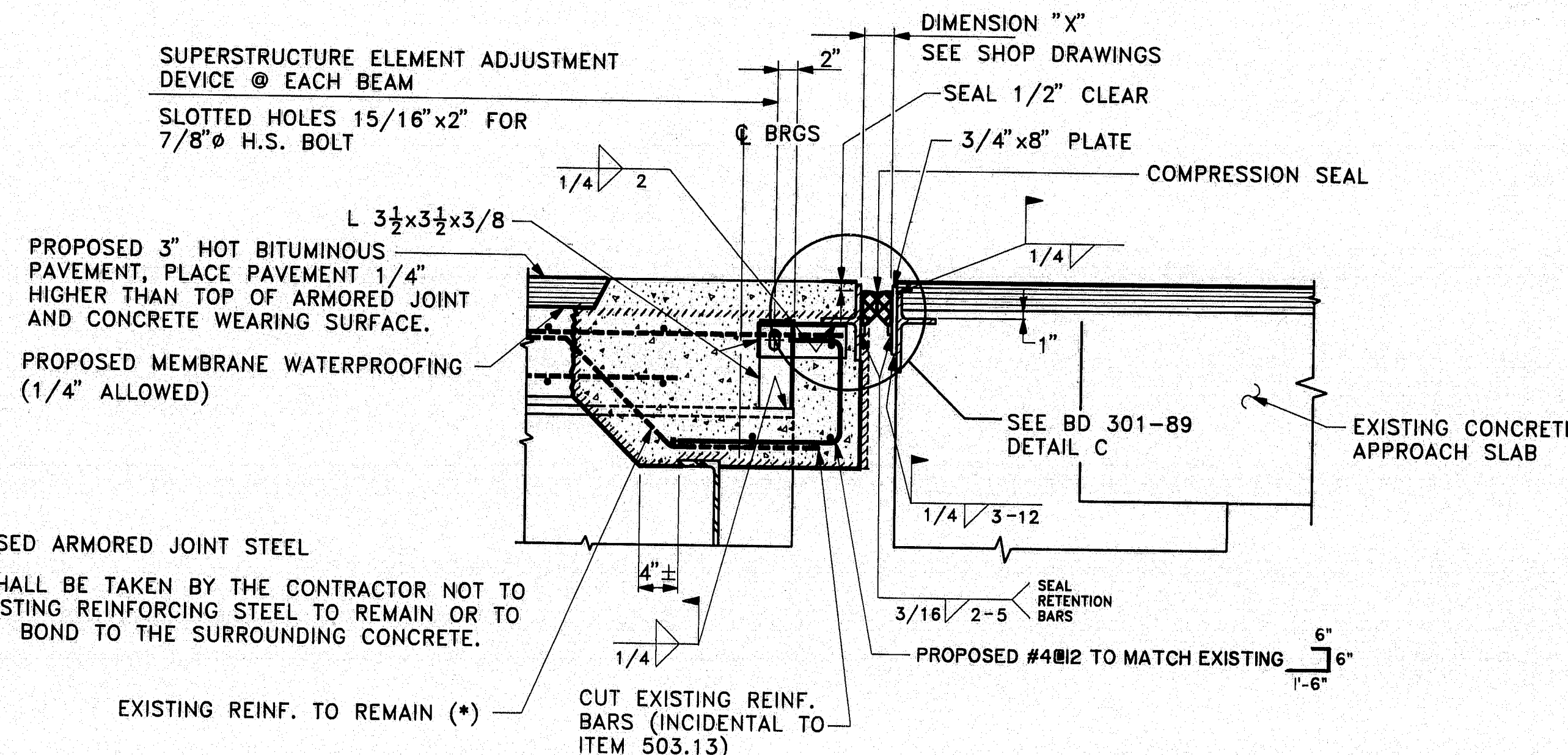
- IF THE DEPTH OF DETERIORATED CONCRETE EXTENDS TO THE REINFORCING BARS, THEN REMOVE CONCRETE TO A MINIMUM DEPTH OF 1" BEYOND THE REINFORCING STEEL.
- RESETTING EXISTING POSTS AND INSTALLATION OF PROPOSED GUARD RAIL POSTS TO BE INCIDENTAL TO ITEM 606.364.
- TERMINAL CONNECTOR AND ATTACHMENTS TO THE EXISTING CONCRETE PARAPET TO BE INCIDENTAL TO ITEM NO. 606.364.
- AFTER THE EXISTING BITUMINOUS PAVEMENT HAS BEEN REMOVED THE CONTR. MAY BE DIRECTED BY THE ENGINEER TO REHABILITATE AREAS OF THE DECK. PAYMENT WILL BE MADE UNDER ITEMS 518.30 OR 518.31 WHICHEVER IS APPLICABLE.
- PROPOSED REINFORCING STEEL SHALL HAVE A MINIMUM COVER OF 2" UNLESS OTHERWISE INDICATED.
- PROTECTIVE COATING FOR CONCRETE SURFACES SHALL BE APPLIED TO ALL EXPOSED SURFACES OF CONCRETE PATCHING AND THE IMMEDIATE SURROUNDING AREA AS DIRECTED BY THE ENGINEER.

SUPERSTRUCTURE ELEMENT ADJUSTMENT DEVICE @ EACH BEAM
SLOTTED HOLES 15/16"x2" FOR 7/8" H.S. BOLT



NOTE:
PAINT EXPOSED ARMORED JOINT STEEL
(SEE STANDARD SHT. BD 301-89 FOR DETAILS NOT SHOWN)

SUPERSTRUCTURE ELEMENT ADJUSTMENT DEVICE @ EACH BEAM
SLOTTED HOLES 15/16"x2" FOR 7/8" H.S. BOLT



(SEE STANDARD SHT. BD 301-89 FOR DETAILS NOT SHOWN)

SYMBOLS

- EXISTING CONCRETE TO BE REMOVED
- NEW CONCRETE

104-859

Pin No. 1881.10
Bridge No. 5830

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

Kimball Chase
ONE GATE STREET
PORTSMOUTH, N.H. 03801
(603) 431-2520

BUCKNAM ROAD
over
I-295
FALMOUTH, MAINE
CUMBERLAND COUNTY
DETAILS & EST. OF QUANTITIES

SHEET 5 OF 7 AUGUST, MAINE August 1989
REVISED AS-BUILT Jeffrey Maden 3/91

GENERAL NOTES

1. THE WORK FOR REPAIRING THE VEHICULAR IMPACT DAMAGED BRIDGE BEAMS SHALL BE PERFORMED IN COMPLIANCE WITH MAINE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, HIGHWAYS AND BRIDGES 1986.
2. ALL NEW STEEL SHALL CONFORM TO AASHTO M 183 (ASTM A36). THE WT 15 X 62 SECTIONS MAY BE CUT FROM A W30 X 124 ROLLED BEAM SECTION OR TEE-SECTIONS MAY BE CONSTRUCTED USING WELDED PLATES OF EQUIVALENT AREA IF APPROVED BY THE ENGINEER.
3. ALL NEW BOLTS SHALL CONFORM TO AASHTO M164 (ASTM A325).
4. TWO 12'-0" WIDE (MIN.) TRAFFIC LANES ON RT-1295 SH SHALL BE MAINTAINED AT ALL TIMES. THE LANES SHALL BE LOCATED BETWEEN THE WORK AREA AND FIER NO. 2, AND BETWEEN FIER NO. 2 AND THE NORTHBOUND LANES. USE 2" HOT BITUMINOUS PAVEMENT AND 18" MIN. THICKNESS OF GRAVEL AND PROVIDE FILL AND DRAINAGE IN THE MEDIAN AREA AS APPROVED BY THE ENGINEER.
5. BEAMS 1 AND 4 SHALL NOT BE REPAIRED SIMULTANEOUSLY. BEAMS SHALL NOT BE SUBJECT TO DIRECT TRAFFIC LOADING DUE TO TRAFFIC OVER THE BUCKNAM ROAD BRIDGE SHALL BE MAINTAINED AS FOLLOWS:
 - A. BEAM 1 REPAIR WORK:
PROVIDE 1 - 12 FOOT WIDE LANE ALONG THE SOUTH SIDE OF BRIDGE, WITH TRAFFIC LIGHTS. THE LANE SHALL BE USED FOR TWO WAY TRAFFIC.
 - B. BEAM 4 REPAIR WORK:
PROVIDE 1 - 12 FOOT WIDE LANE ALONG THE NORTH SIDE OF BRIDGE, WITH TRAFFIC LIGHTS. THE LANE SHALL BE USED FOR TWO WAY TRAFFIC.
6. THE SECS OF BEAMS 1 AND 4 SHALL BE HEAT STRAIGHTENED AFTER THE DAMAGED SECTIONS OF THE BEAMS HAVE BEEN REMOVED. THE HORIZONTAL WEB CUT SHALL BE MADE WITH A GUIDED TORCH. THE CUT MUST BE STRAIGHT.
7. THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR ALL NEW STRUCTURAL STEEL, DETAILS OF JACKING FRAME, AND A DETAILED CONSTRUCTION PROCEDURE TO THE MAINE DEPARTMENT OF TRANSPORTATION FOR APPROVAL. NO WORK ON BEAM REPAIR SHALL COMMENCE UNTIL THE PLANS AND MATERIALS ARE APPROVED.

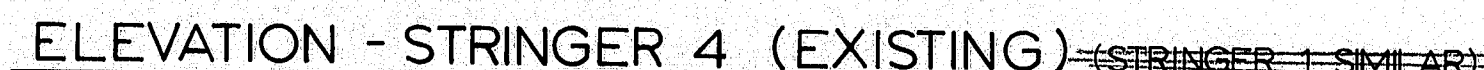
NOTES

1. ALL SPLICE CONTACT AREAS ON EXISTING STEEL MUST BE BLAST CLEANED TO BARE METAL BEFORE SPlice PLATES ARE INSTALLED.
2. ALL WELDING SHALL CONFORM TO AWS D1.1-80 SPECIFICATIONS, AND AASHTO WELDING SPECIFICATIONS 18R.
3. MATCH MARK ALL STEEL REMOVED THAT IS TO BE RE-ASSEMBLED.
4. PAYMENT FOR THE WORK AND MATERIALS REQUIRED FOR THE BEAM REPAIRS, EXCEPT FOR PAINTING THE STEEL, WILL BE PAID UNDER ITEMS 504.70 AND 504.71.
5. THE WORK AND MATERIALS REQUIRED FOR CLEANING AND PAINTING THE PROPOSED STEEL AND FOR THE REPAIRED OR RE-ASSEMBLED STEEL WILL BE CONSIDERED INCIDENTAL TO ITEM 506.142-FIELD PAINTING EXISTING STRUCTURAL STEEL.
6. ~~PAYMENT FOR MAINTENANCE OF TRAFFIC WILL BE MADE UNDER SUBSTRUCTURE ITEMS, EXCEPT PAVERING AND REMOVING THE FILL AND DRAINAGE CURBING AND REPAIRING THE EXISTING DRIVEWAY AND SIDEWALKS. GRAVEL WILL BE CONSIDERED INCIDENTAL TO CURBMENT AND SIDEWALKS.~~



JACKING NOTES:

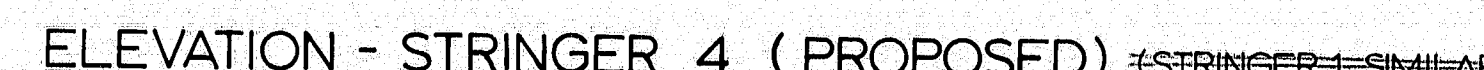
1. THE JACKING FRAME AND TEMP JACKING BEAM OF THE CONTRACTOR'S DESIGN SHALL BE SUBMITTED FOR THE ENGINEER'S APPROVAL PRIOR TO PERFORMING THE WORK.
2. MIN. 50 TON JACKS REQUIRED.



NOTE: DIAPHRAGM ANGLES SHALL BE INSTALLED PRIOR TO REMOVING ADJACENT DAMAGED EXISTING DIAPHRAGM TO FASCIA BEAM.



Δ1 - Note change - 8.25.89
 Δ2 - Beam 1 12.12.89
 See Sheet #7



PROPOSED DIAPHRAGM CONNECTION



PLANS

Pin No. 1881.10
Bridge No. 5830

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

 Kimball Chase ONE CATE STREET
PORTSMOUTH, N.H. 0380
(603) 431-2520

BUCKNAM ROAD
over

I-295
FALMOUTH, MAINE
CUMBERLAND COUNTY
BEAM REPAIR DETAILS

SHEET 6 OF 7 AUGUSTA, MAINE August 1888

REVISIO AS-BUILT Jeffrey Madson 3/9

ELEVATION

1/2" GAP MAX FOR PROPOSED MATERIAL (TYP.)

EXIST. SPLICE PLATES TO REMAIN

MAINTAIN 1/2" GAP MAX FOR PROPOSED WT 15x62 (TYP)

EXIST. SPLICE PLATES TO BE REMOVED TO FACILITATE INSTALLATION OF PROP. WT 15x62. REUSE EXIST. SPLICE PLATES AND RECONNECT WITH NEW BOLTS

DO NOT REMOVE

EXIST. STRINGER

EXIST. FILL

PROPOSED WT 15x62

Remove Existing Beam Install Proposed Beams

ELEVATION

EXIST. STRINGER

PROPOSED WT 15x62

PROPOSED PL. 24"x19"x5/8" BOTH SIDES

7/8" Ø BOLTS

PROPOSED WT 15x62

BAR 4"x3/4" x 2'-7" LG. EA. SIDE

PROPOSED PL. 5/8"x10" x 2'-7" LG.

1'-7"

1 1/2"

3" 3" 2" 3" 3"

C CUT

7 SPCS @ 3"

2'-0"

1/2" MAX. GAP (TYP.)

ELEVATION

EXIST. STRINGER

PROPOSED WT 15x62

PROPOSED BAR 3"x3/8" x 4'-1" LG. EA. SIDE

PROPOSED PL. 1"x8" x 4'-1" LG.

EXIST. 1/2"x8" COVER PLATE

1/2"x8" PROPOSED COVER PL. TO REPLACE EXIST.

SEE PROPOSED SPLICE (BEAM 1) FOR WEB SPLICE DETAILS

SECTION

EXIST. STRINGER

PROP. WT 15

REUSE EXIST. SPLICE PLATE (TOP AND BOT.)

SECTION

EXIST. STRINGER

PROP. WT 15

REUSE EXIST. SPLICE PLATE (TOP AND BOT.)

SECTION

PROPOSED WT 15

7/8" Ø BOLTS (TYP.)

EXIST. STRINGER

1 1/2"

4 SPCS @ 3" 2' 2" 4 SPCS @ 3"

2'-7"

1 1/2"

SECTION

PROPOSED WT 15

7/8" Ø BOLTS (TYP.)

EXIST. STRINGER

2 1/2"

2 1/2"

1 1/2"

7 SPCS @ 3" 2' 2" 7 SPCS @ 3"

4'-1"

1 1/2"

EXIST. SPLICE DETAILS BEAM 1

EXIST. SPLICE DETAILS BEAM 4

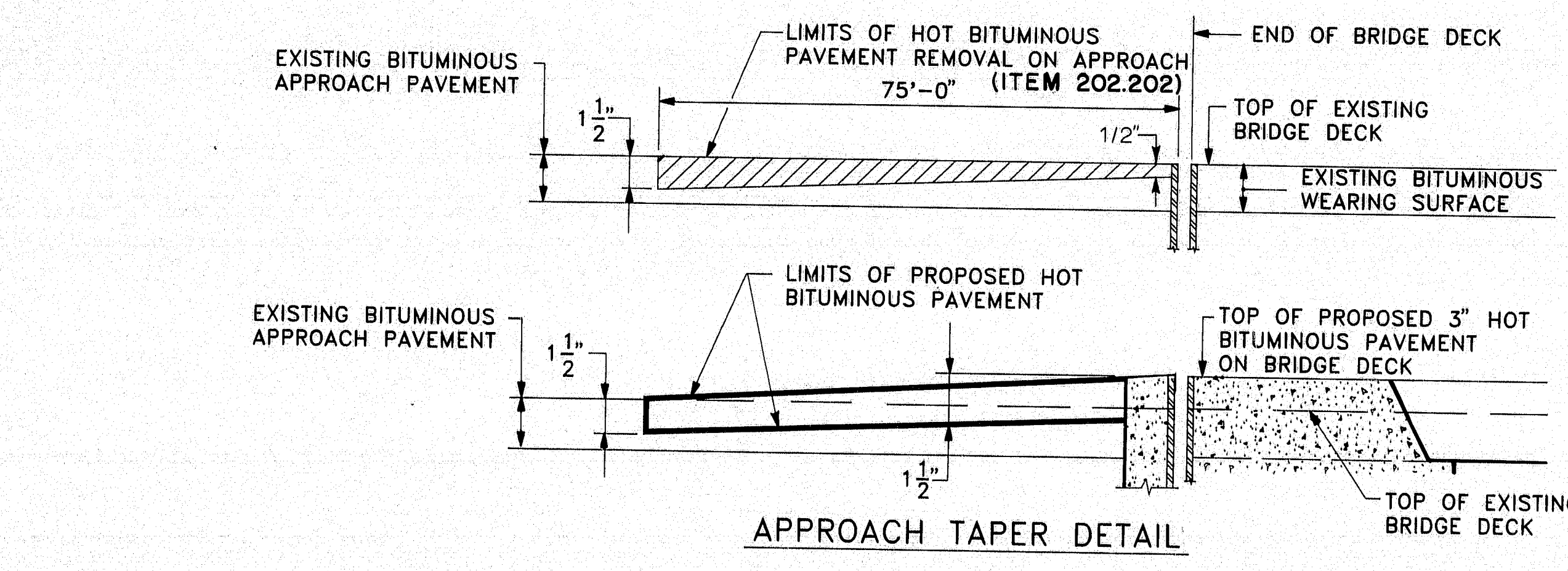
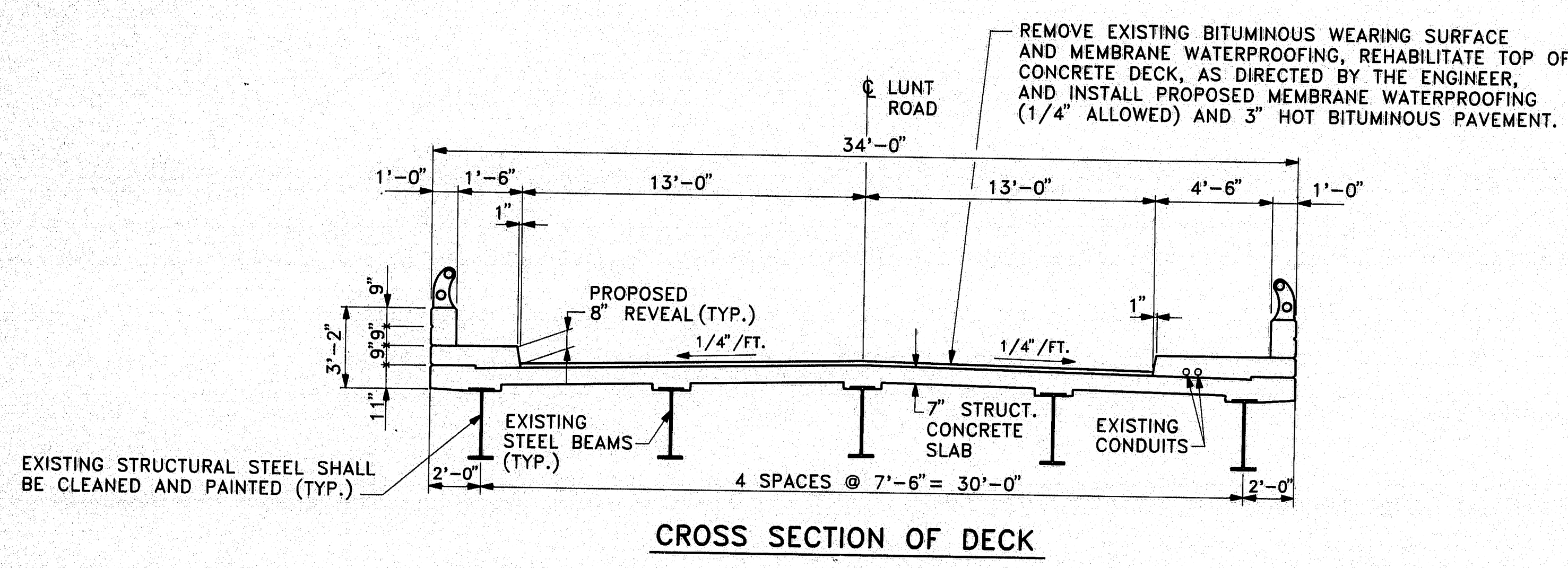
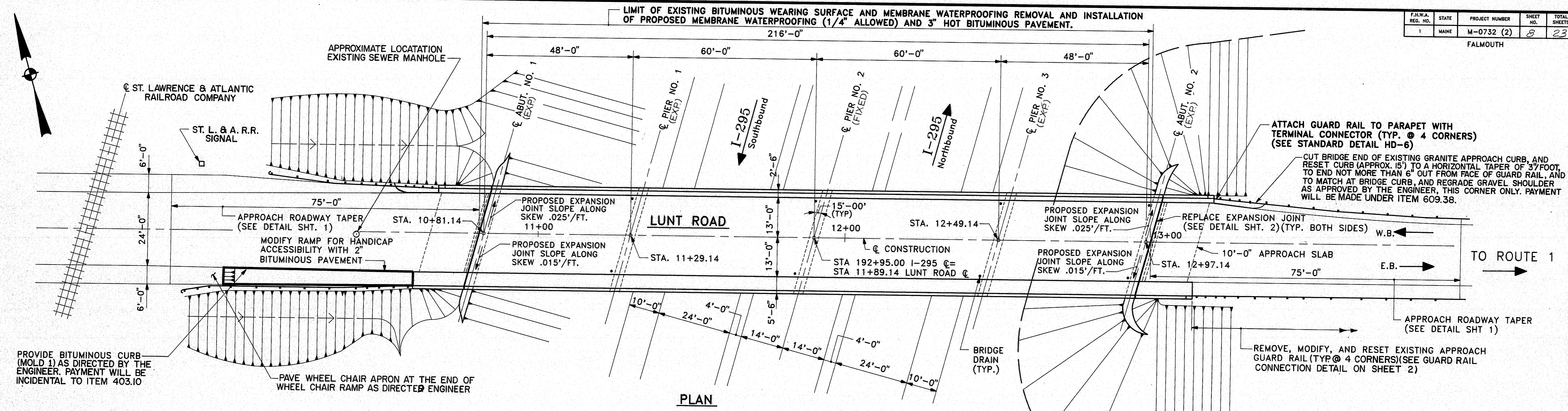
PROPOSED SPLICE DETAILS BEAM 1

PROPOSED SPLICE DETAILS BEAM 4

+ EXISTING HOLES

REVISED AS-BUILT Jeffrey Madson 3/91

PLANS	PROJECT DESIGN ENGINEER		BY	SEM	DATE
	DESIGN-	DETAILS	JJF	JEB	5-89
	CHECKED		SEM/DLT		6-89
	REVISIONS				



SCOPE OF WORK

- REMOVE AND REPLACE EXISTING BITUMINOUS CONCRETE WEARING SURFACE AND MEMBRANE WATERPROOFING WITH A PROPOSED MEMBRANE WATERPROOFING (1/4\"/>

SPECIFICATION

DESIGN: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES 1983 AND INTERIM SPECIFICATIONS THRU 1988.
 CONTRACT: STATE OF MAINE, DEPARTMENT OF TRANSPORTATION, STANDARD SPECIFICATIONS, HIGHWAYS AND BRIDGES, REVISION OF JULY 1988.

DESIGN LOADING

LIVE LOAD: H20-44 (DESIGNED)

MATERIALS

CONCRETE: CLASS A
 REINFORCING STEEL: ASTM A615 GRADE 60
 STRUCTURAL STEEL: ASTM A36 (PAINTED)
 HIGH STRENGTH BOLTS: ASTM A325, TYPE 3

BASIC ALLOWABLE STRESSES

CONCRETE: $f_c = 1200 \text{ psi}$
 REINFORCING STEEL: $f_s = 24,000 \text{ psi}$
 STRUCTURAL STEEL: ASTM A36 $f_s = 18,000 \text{ psi}$

TRAFFIC DATA

AADT (1986) 3,230
 AADT (2006) 3,880
 DESIGN HOURLY VOLUME 466
 PERCENT TRUCKS 3
 DIRECTIONAL DISTRIBUTION (%) 55
 DESIGN SPEED (MPH) 35
 18 KIP EQUIVALENT P2.5 24

UTILITIES

N.E.T. CONDUITS LOCATED WITHIN SIDEWALK ST. LAWRENCE & ATLANTIC RAILROAD

PLANS OF THE EXISTING BRIDGE ARE AVAILABLE FOR THE CONTRACTOR'S REFERENCE AT THE BRIDGE DESIGN OFFICE IN AUGUSTA. THE PLANS ARE REPRODUCTIONS OF ORIGINAL DRAWINGS AS PREPARED FOR THE CONSTRUCTION OF THE BRIDGE AND IT IS VERY UNLIKELY THAT THE PLANS WILL SHOW ANY CONSTRUCTION FIELD CHANGES OR ANY ALTERATIONS WHICH MAY HAVE BEEN MADE TO THE BRIDGE DURING ITS LIFE SPAN.

LUNT ROAD INDEX OF SHEETS

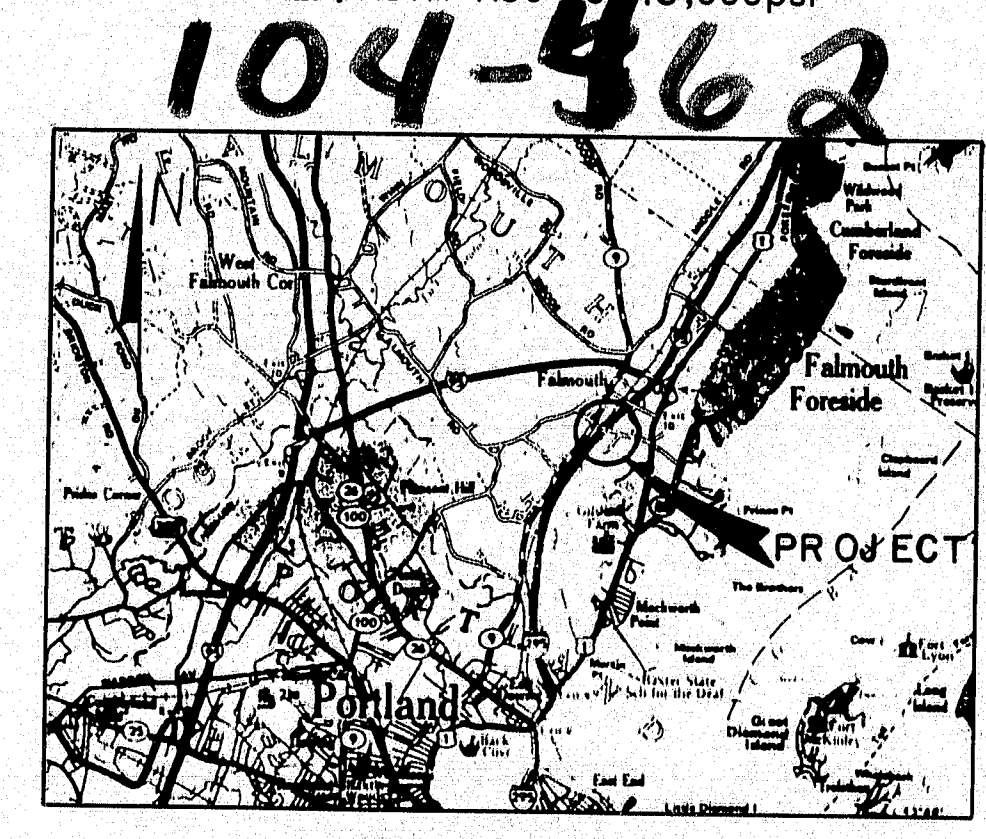
GENERAL PLAN 1
 DETAILS AND EST. OF QUANTITIES . 2

BRIDGE STANDARD DETAILS

BD 301-89 - EXPANSION DEVICE
 BD 201-89 - CONCRETE END POST

HIGHWAY STANDARD DETAILS

HD-6 TYPE 3 GUARD RAIL
 HD-14 CURB CUT RAMPS



LOCATION MAP
 SCALE

Pin No. 2811.00
 Bridge No. 5829

STATE OF MAINE
 DEPARTMENT OF TRANSPORTATION

Kimball Chase ONE GATE STREET
 PORTSMOUTH, N.H. 03801
 (603) 431-2520

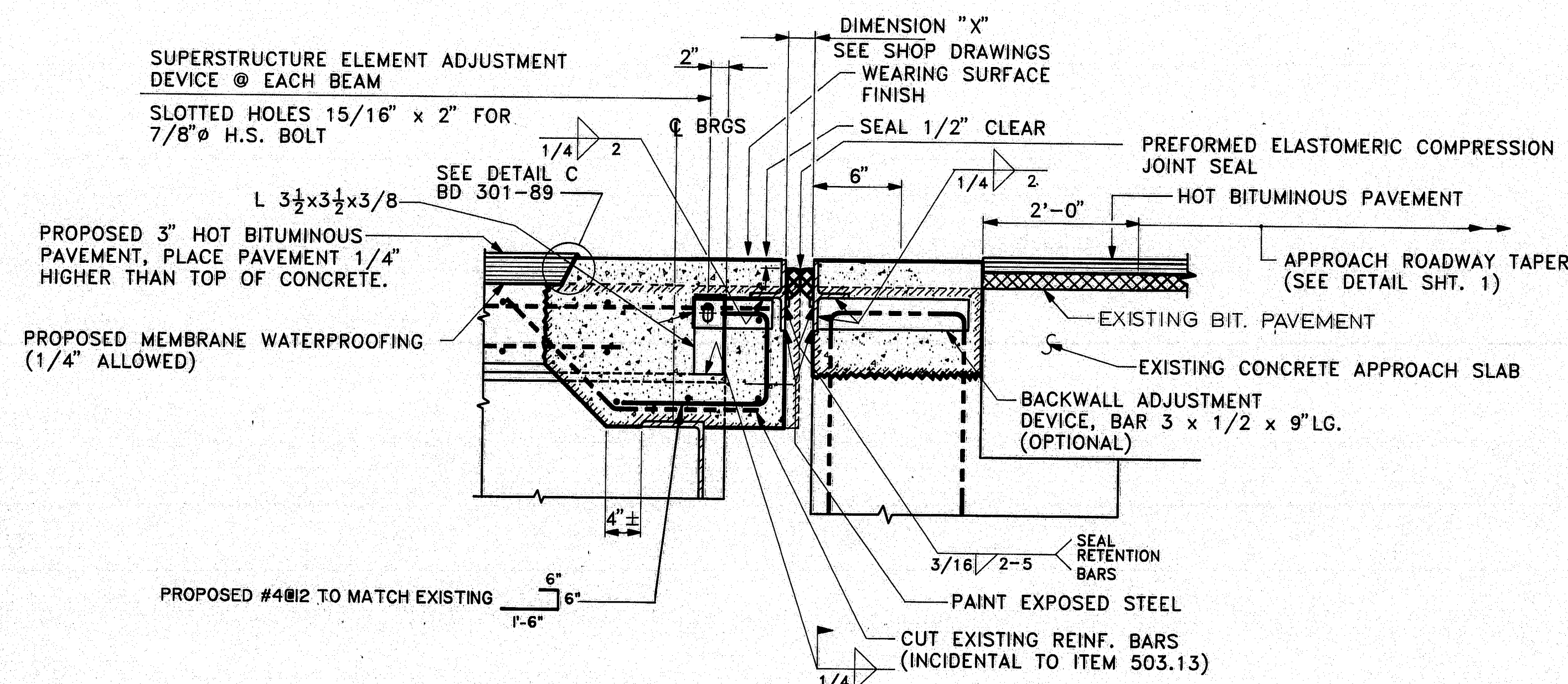
LUNT ROAD
 over
 I-295
 FALMOUTH, MAINE
 CUMBERLAND COUNTY
 GENERAL PLAN

SHEET 1 OF 2 AUGUSTA, MAINE August 1989

Revised As-Built Jeffrey Madison 3/91

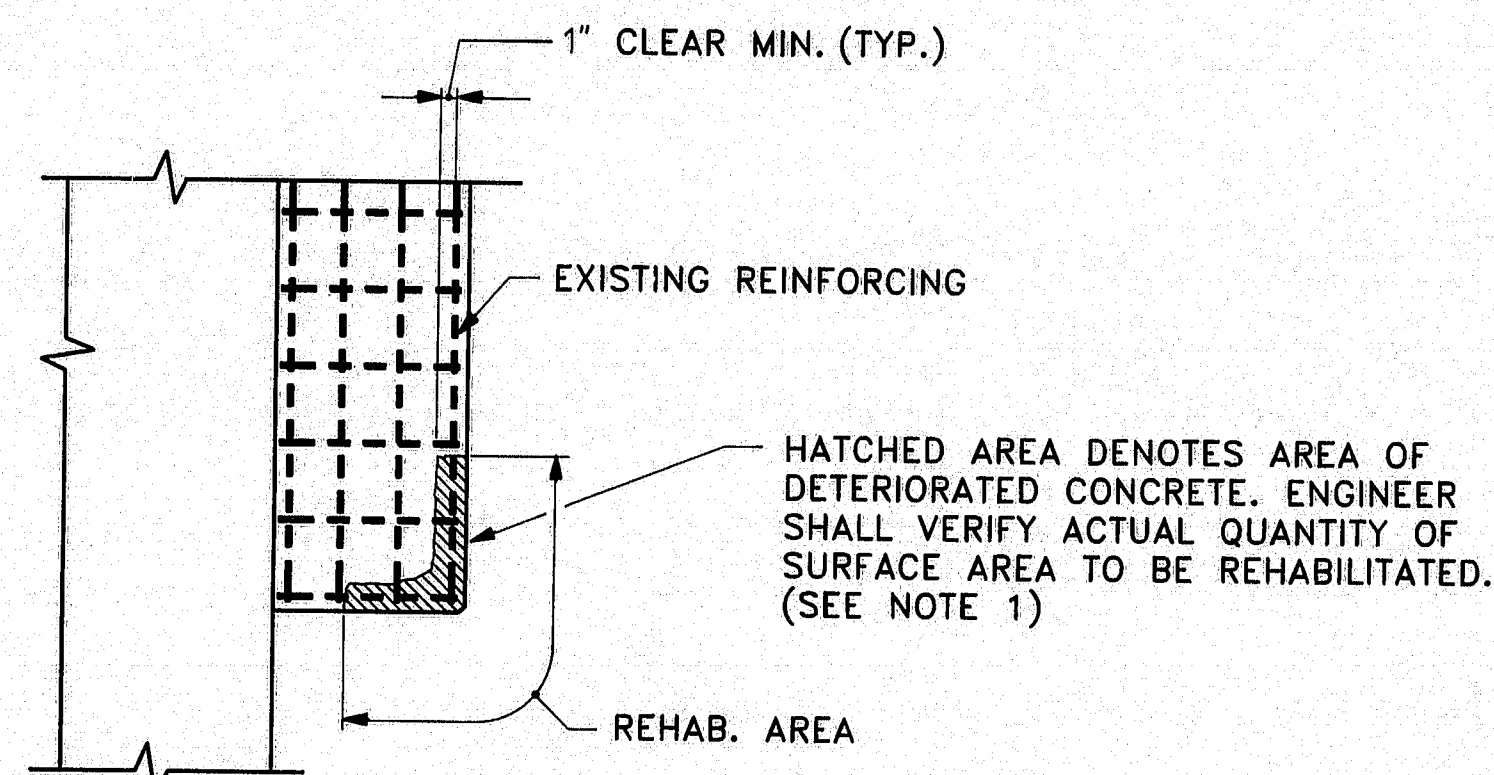
PROJECT DESIGN ENGINEER	BY	SEM	DATE
PLANS	DESIGN	JUL	5-87
	DETAILED	JEB	
	CHECKED	SEM/DLT	6-87
	REVISIONS		
	FIELD CHANGES		

ESTIMATED QUANTITIES			
ITEM NO.	DESCRIPTION	QUANTITY	UNIT
202.127	REMOVAL OF EXISTING BITUMINOUS PAVEMENT	0.23	L.S.
202.202	REMOVING PAVEMENT SURFACE	475	S.Y.
403.10	HOT BITUMINOUS PAVEMENT, GRADING D	142	TON
503.12	REINFORCING STEEL, FABRICATED AND DELIVERED	220	lbs.
503.13	REINFORCING STEEL, PLACING	220	lbs.
506.142	FIELD PAINTING EXISTING STRUCTURAL STEEL	0.23	L.S.
508.13	MEMBRANE WATERPROOFING	0.23	L.S.
515.21	PROTECTIVE COATING FOR CONCRETE SURFACE	0.03	L.S.
518.21	REHAB. OF STRUCTURAL CONCRETE SUBSTRUCTURE	124	S.F.
518.30	REHAB. OF STRUCT. CONC. SLAB-TO REINFORCING STEEL	283	S.F.
518.31	REHAB. OF STRUCT. CONC. SLAB TO BELOW REINF. STEEL	113	S.F.
518.34	REHAB. OF CONC. CURBS & SIDEWALKS (INCL. SURFACES)	160	S.F.
518.35	REHAB. OF CONC. CURBS & SIDEWALKS (HORIZ. SURFACES)	220	S.F.
520.2402	BRIDGE JOINT MODIFICATION - LUNT ROAD	2	EA
606.35	GUARD RAIL DELINEATOR POST	4	EA
606.364	GUARD RAIL, REMOVE, MODIFY AND RESET	675	L.F.
606.751	WIDEN SHOULDER FOR BREAKAWAY CABLE TERMINAL	4	EA
606.77	BREAKAWAY CABLE TERMINAL	4	EA
609.38	RESET CURB TYPE I	15	L.F.
627.63	4 INCH SOLID YELLOW PAVEMENT MARKING LINE	750	L.F.
627.65	WHITE OR YELLOW PAVEMENT AND CURB MARKING	25	S.F.
627.67	REMOVING PAVEMENT MARKINGS	150	S.F.
627.68	TEMP. 4" PAINTED PAVE MARKING LINE, YELLOW OR WHITE	750	L.F.
639.19	FIELD OFFICE TYPE B	0.2	EA
6437202	TEMPORARY TRAFFIC SIGNAL - LUNT ROAD	1	L.S.
652.310	TYPE I BARRICADE	15	EA
652.311	TYPE II BARRICADE	3	EA
652.330	DRUM	10	EA
652.340	CONE	3	EA
652.350	CONSTRUCTION SIGNS	165	S.F.
652.361	MAINTENANCE OF TRAFFIC CONTROL DEVICES	0.17	L.S.
652.380	FLAGGER	200	MH
659.10	MOBILIZATION	0.17	L.S.

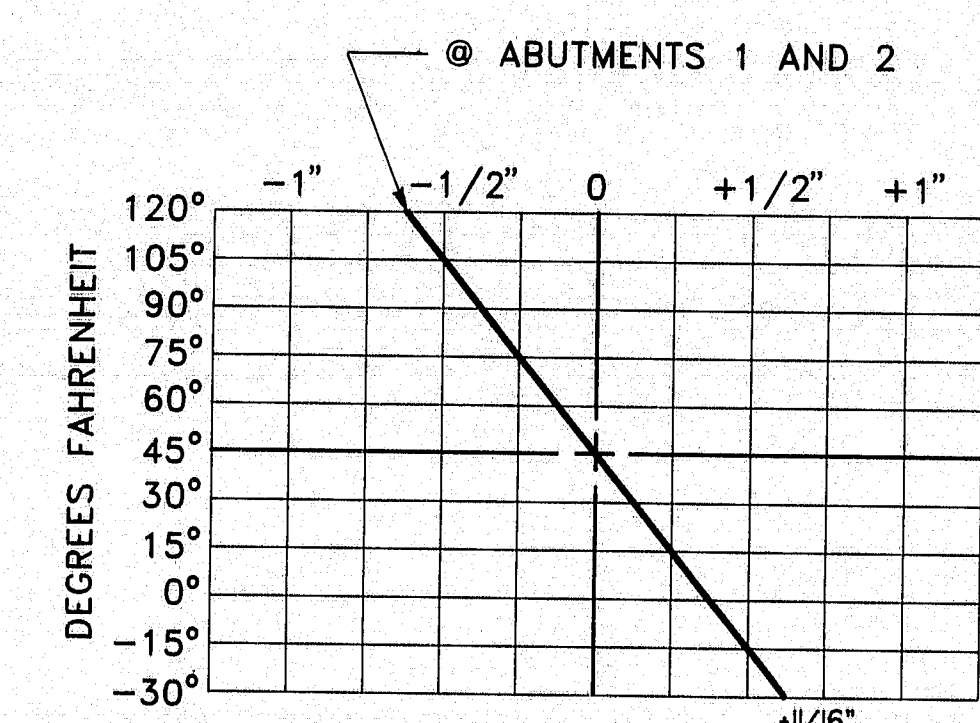
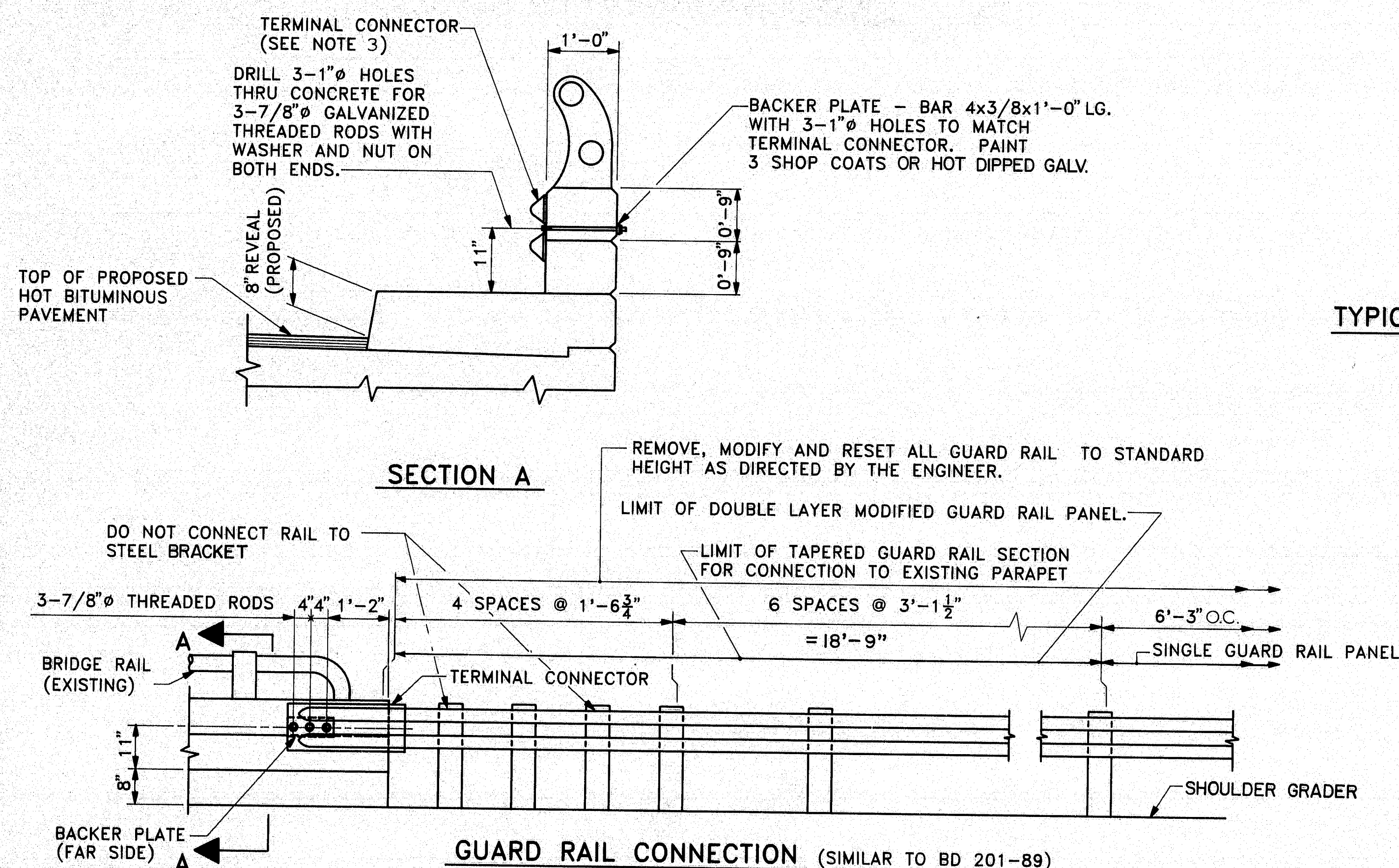


EXPANSION JOINT DETAIL

(SEE STANDARD SHT. BD125-82
FOR DETAILS NOT SHOWN)



TYPICAL ABUTMENT REHABILITATION - PLAN



COMPRESSION SEAL ADJUSTMENT CHART

THE TEMPORARY SIGNAL CONTROLLER SHALL BE A TWO-PHASE PRETIMED CONTROLLER. IT SHALL OPERATE AS SHOWN BELOW.

SEQUENCE OF OPERATION							
INTERVAL	1	2	3	4	5	6	7
HEADS							
EB	G	Y	R	R	R	R	G
WB	R	R	R	G	Y	R	Y
TIMING 60 SECOND DIAL	17	3	10	17	3	10	

WHERE
G=GREEN
Y=YELLOW
R=RED


SYMBOLS

 EXISTING CONCRETE TO BE REMOVED
 NEW CONCRETE

104-463

Pin No. 2811.00
Bridge No. 5829

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

 **Kimball Chase** ONE CATE STREET
PORTSMOUTH, N.H. 03801
(603) 431-2520

LUNT ROAD

over

I-295

FALMOUTH, MAINE

CUMBERLAND COUNTY

DETAILS & EST. QUATITIES

SHEET 2 OF 2 AUGUSTA, MAINE August 1989

REVISED AS-BUILT Jeffrey McAdams 3/91